

Everett Link Extension & OMF North

M2023-47

System Expansion Committee

06/08/23



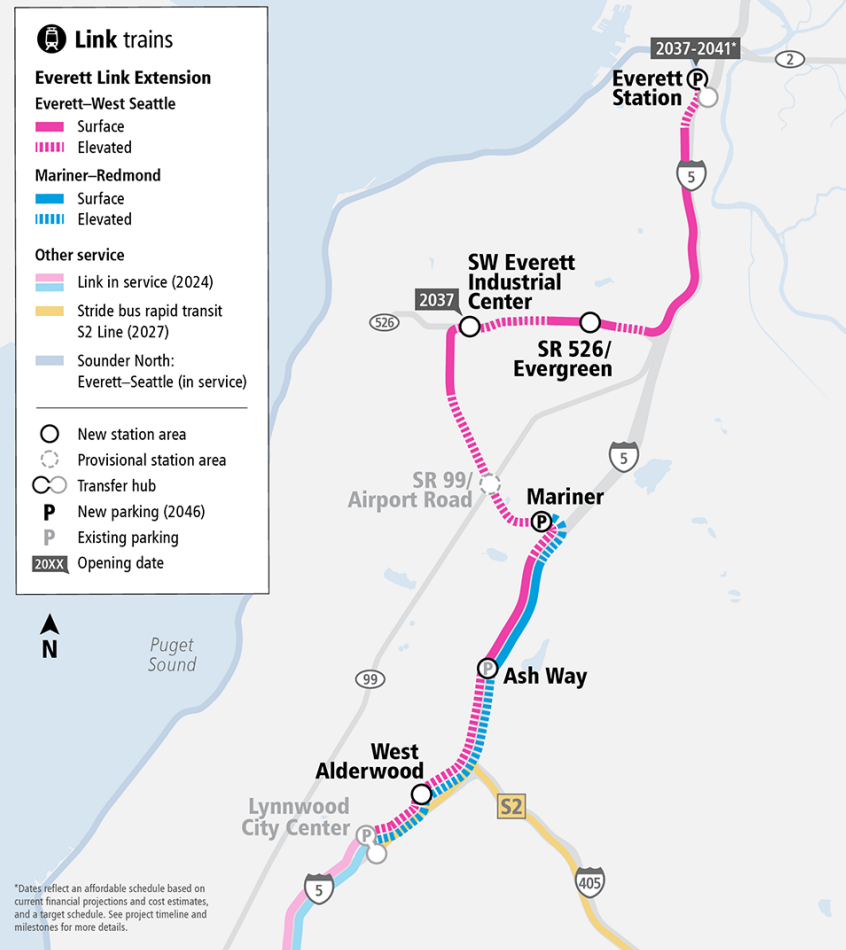
Why we are here

- Review Community Advisory Group and Elected Leadership Group recommendations for alternatives to study in the Draft EIS and preferred alternative.
- Review staff recommendations, requested by the Board.
- Consider forwarding Motion 2023-47 to the Board for approval.

Everett Link Extension

ST3 Representative Project

- **Length:** 16 miles
- **Stations:** six plus one provisional (unfunded) station
- **Target schedule:** 2037
- **Affordable schedule:** 2037/2041
- **New parking** at Everett Station and Mariner opens 2046



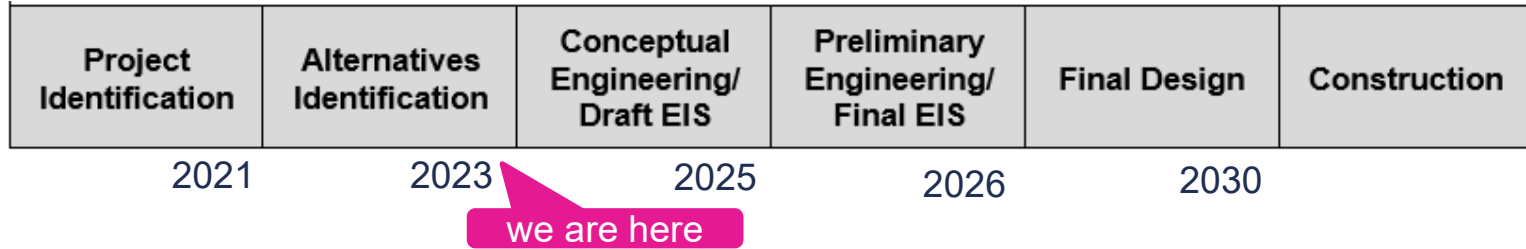
Operations and Maintenance Facility North

Facility supports EVLE and system-wide expansion needs

- **Capacity:**
 - Store, maintain, and repair vehicles
 - 150+ light rail vehicles
 - 450+ high skilled, living wage jobs
- **Affordable and Target Schedules: 2034**






Tracking to the Affordable Schedule



Project Section	Target Schedule	Affordable Schedule
OMF North	2034	2034
Lynnwood to SW Everett Industrial Center	2037	2037
SW Everett Industrial Center to Everett Station	2037	2041
Parking	2046	2046

Project Performance Tracker Status

Cost Risk 	Schedule Risk (to next milestone) 	Schedule Risk (to delivery date) 
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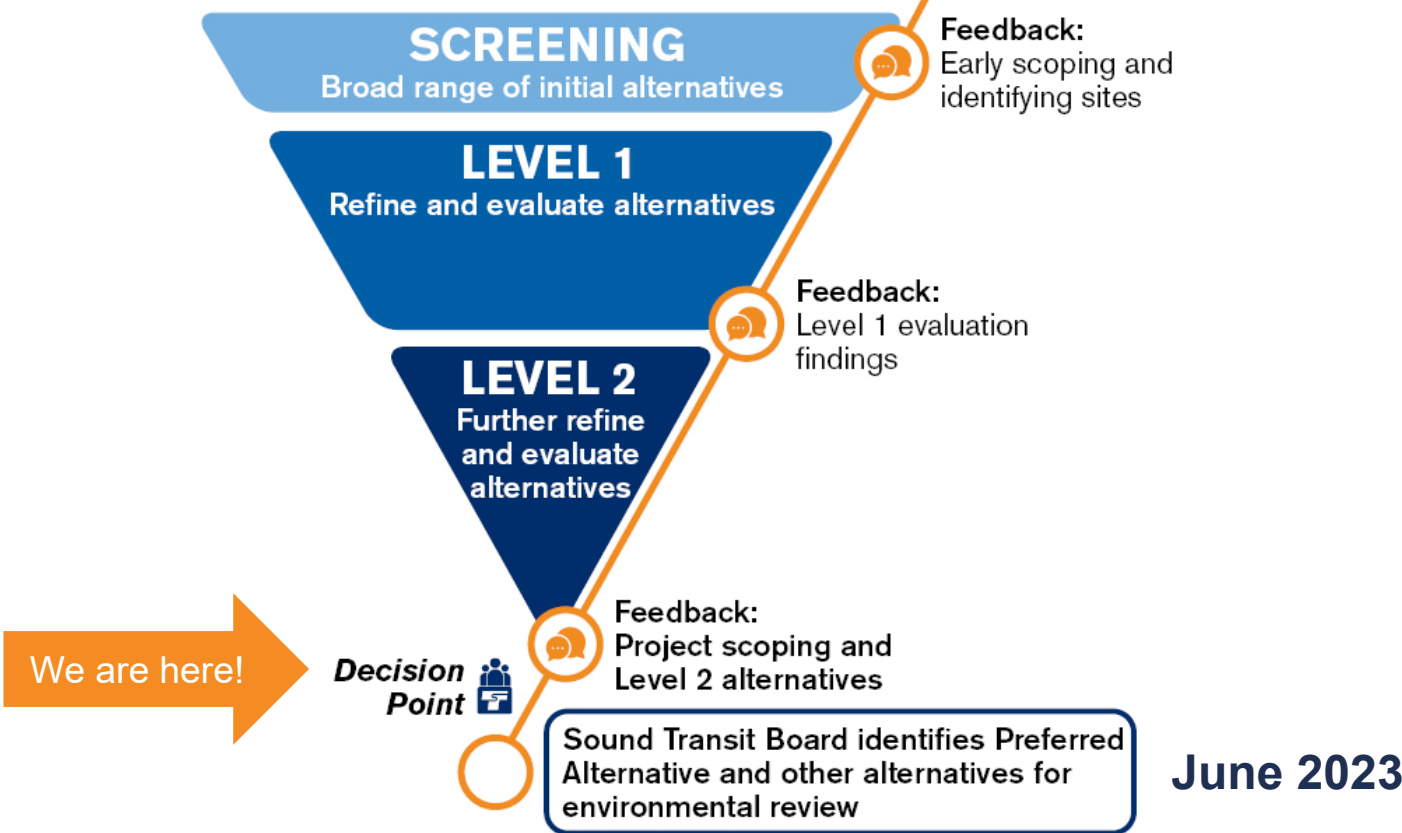
Cost Risks

- Comparative cost estimates are within an acceptable range; Quantitative Risk Assessment held in Q1 2023.
- Unknown engineering challenges in early design.
- Unknown ROW needs in early design.

Schedule Risks

- Emerging risks due to delays to environmental review process with 3rd parties.
- Design is at 1-2% so many unknowns exist.
- Property acquisition risks due to yet unknown ROW needs.
- Delivery method not yet selected.

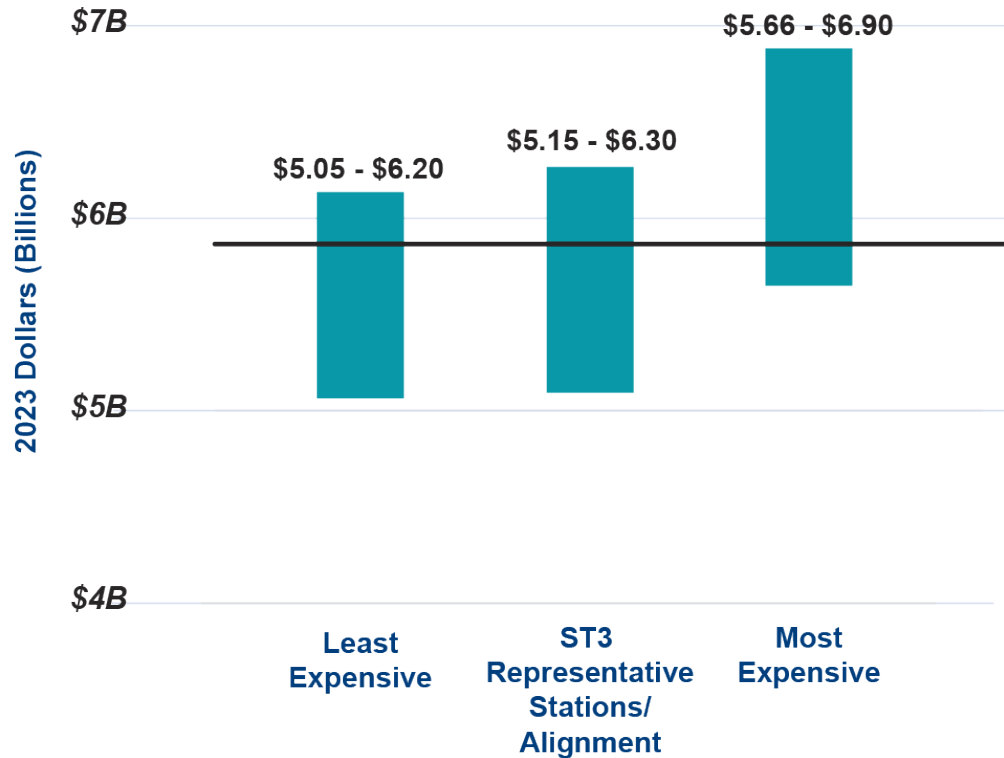
Alternatives Development Process



Preliminary Comparative Cost Estimates

- **Inform comparison** of alternatives
- Based on **limited conceptual design** (1-2% design)
- Does not establish project budget, budget established at baselining
- Comparative cost estimates for the OMF North and end-to-end alignments are presented in a range of -2% to +20%
- The range is based on Sound Transit experience and the Association for Advancement of Cost Engineering International (AACEi) industry standards
- Reviewed by Independent Cost Consultant, TriUnity; no changes recommended for this phase

EVLE: Preliminary Comparative Cost Estimate*



ST Realigned Target Financial Plan Estimate \$5.90B (2023\$)

Estimate Range (-2% to +20%)

****Estimates are to be used for comparisons among alternatives only.***

Note: Estimates do not include parking or full design or construction of the SR 99/Airport Rd provisional station.

OMF North: Preliminary Comparative Cost Estimate*



ST Realigned Financial Plan Estimate \$1.53B
(2023\$)

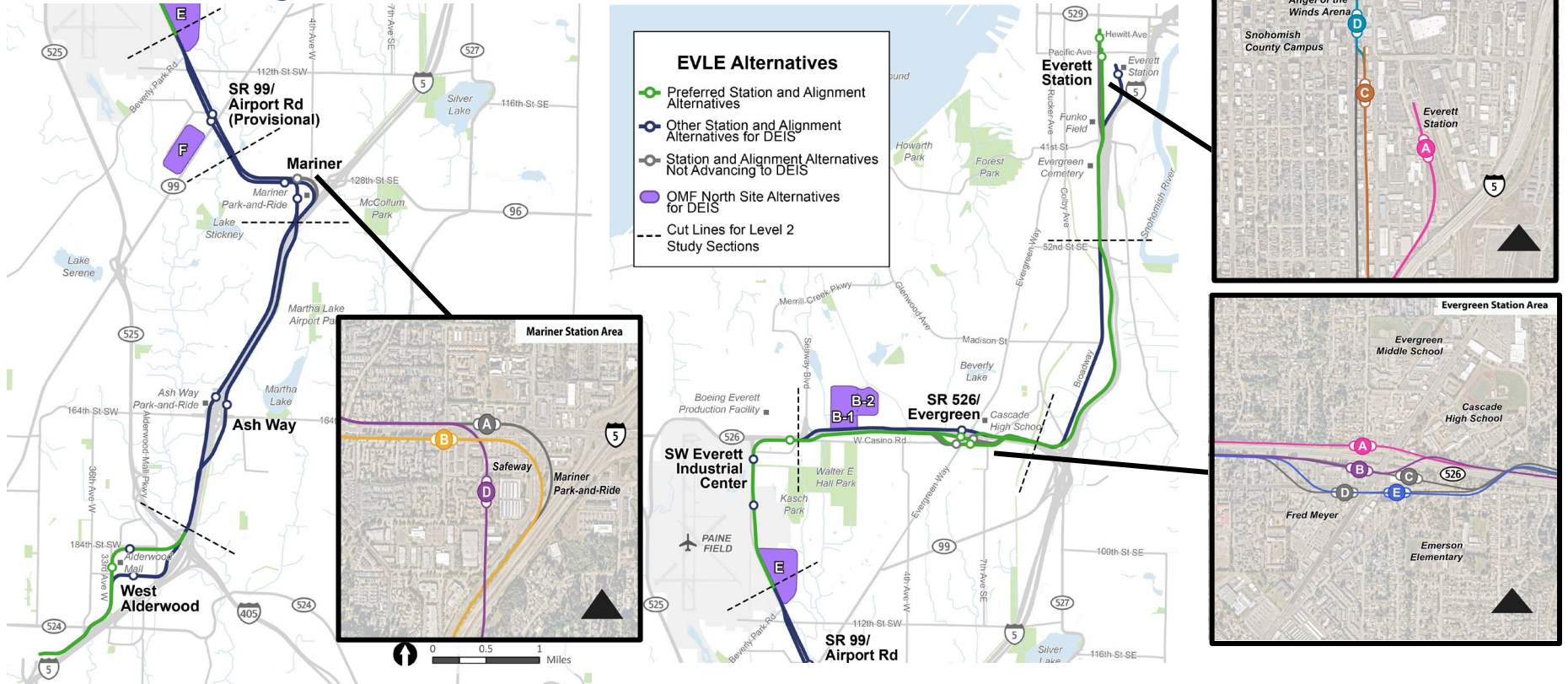
 Estimate Range (-2% to +20%)

**Estimates are to be used for comparisons among alternatives only.*

Today's Action – M2023-47

Identifies the preferred alternative and other alternatives for the EVLE Draft EIS for all segments, consistent with Elected Leadership Group and Staff Recommendations.

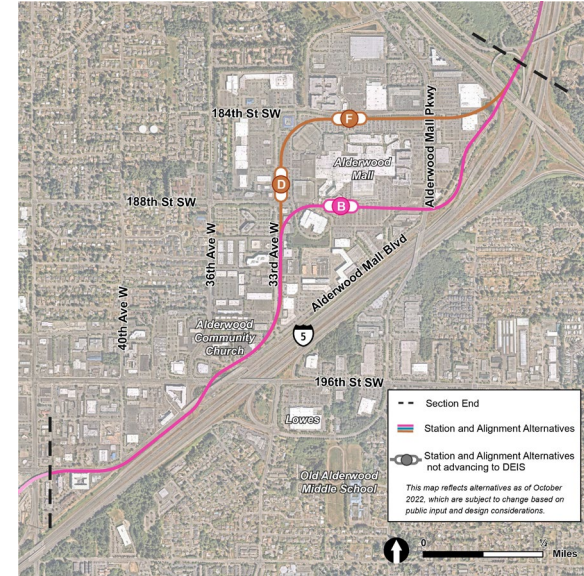
Today's Action – M2023-47



West Alderwood

Recommendations

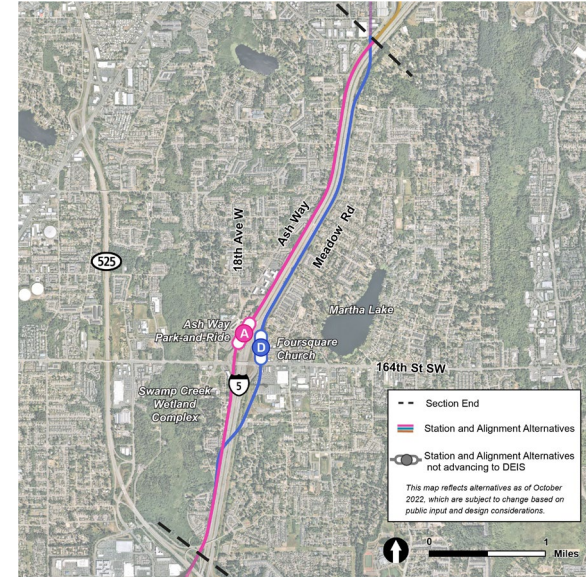
	CAG	ELG	ST Staff
B	Remove from study Limited TOD potential and serves fewer historically underserved communities	Remove from study/ continue if needed Not favored, only study if needed for a reasonable range of alternatives	Continue to study ST staff recommends continuing to study to retain multiple options in the DEIS.
D	Preferred Stimulates TOD, closest to serve both residents and businesses	Preferred Concur with CAG recommendations	Preferred Best connections to Swift BRT, serves most historically underserved communities
F	Continue study Close to the mall and other businesses, better foot traffic	Continue study Concur with CAG recommendations	Continue study Most potential for new development, easier to walk and bike to



Ash Way

Recommendations

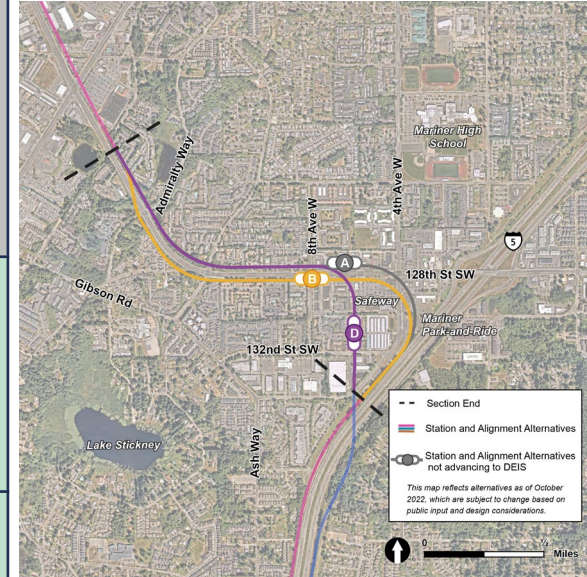
	CAG	ELG	ST Staff
A	<p>Continue to study Connected to park-and-ride, better access for existing residents</p>	<p>Continue to study Both alternatives should continue to be studied</p>	<p>Continue to study Serves more historically underserved communities, best connection to Swift BRT</p>
D	<p>Preferred Closer to Interurban Trail and shopping areas, opportunity for future development</p>	<p>Continue to study Both alternatives should continue to be studied</p>	<p>Continue to study More potential for new development, most aligned with local planning</p>



Mariner

Recommendations

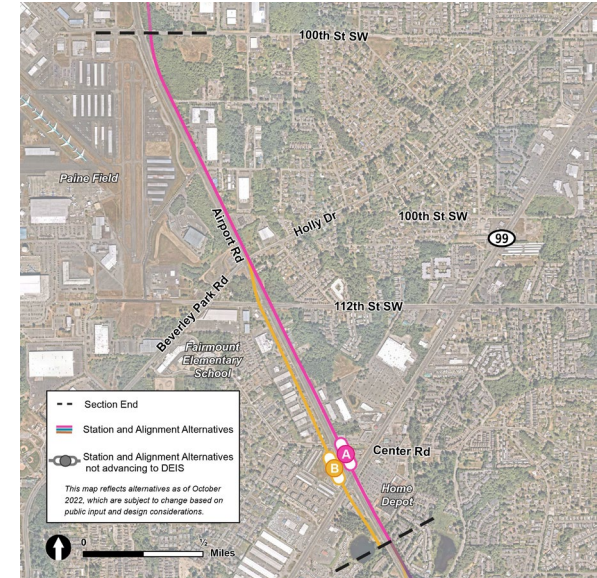
	CAG	ELG	ST Staff
A	Remove from study Concerns about residential displacements; similar to Alt. B but more challenges	Remove from study Concur with CAG recommendation	Remove from study Serves fewer historically underserved communities and similar impacts as Alt. B
B	Preferred Better access to businesses and schools, TOD potential, fewer displacements	Continue study Concur with CAG recommendation	Continue study Highest planned population and job growth, serves most historically underserved communities
D	Continue study Closest to park-and-ride, consistent with Snohomish County access plans	Continue study Concur with CAG recommendation	Continue study Most potential for new development, most aligned with local planning



SR 99/Airport Road

Recommendations

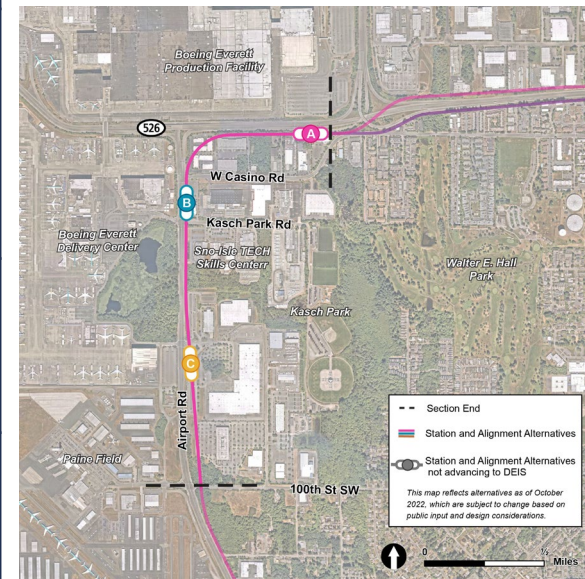
	CAG	ELG	ST Staff
A	Preferred Convenient for surrounding residential areas, perceived opportunity for TOD development	Continue to study	Continue to study Better connection to Swift BRT, less disruptive to business access during construction
B	Continue to study Convenient connections to existing transit	Continue to study Concur with CAG recommendation	Continue to study Easier to pick-up and drop-off, more potential for new development adjacent to the station



SW Everett Industrial Center

Recommendations

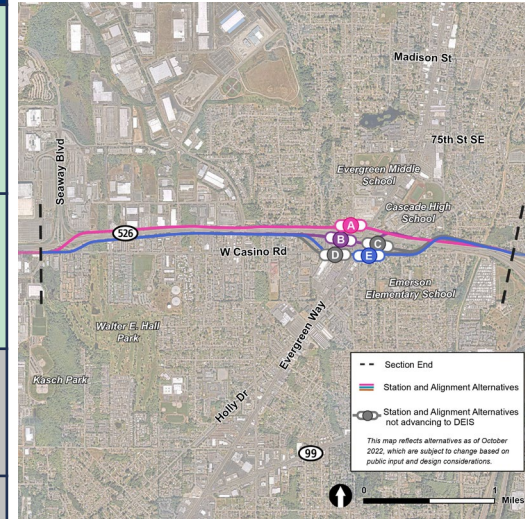
	CAG	ELG	ST Staff
A	Mixed Best connection to Boeing, closer to Casino Rd residents but with potential impacts	Preferred Connects to Casino Rd residents, best connection to Boeing	Preferred Connection to Boeing and regional employment, serves historically underserved communities
B	Mixed Easier to integrate with Swift, no residential nearby	Continue study Reasonable range of alternatives	Continue study Shorter travel times for buses
C	Mixed Could support Holly and Westmont neighborhoods, closest to airport	Remove from study/continue if needed* Not favored, only study if needed for a reasonable range of alternatives	Continue study ST staff recommends continuing to study to retain multiple station options in the DEIS.



SR 526/Evergreen

Recommendations

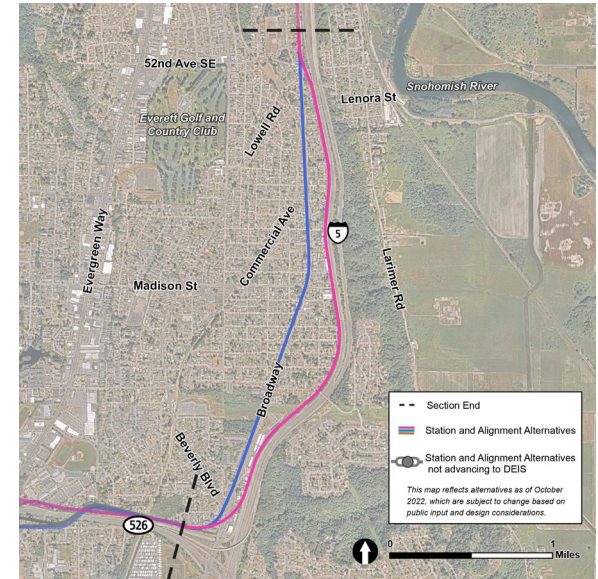
	CAG	ELG	ST Staff
A	Mixed Near new affordable housing development, poor transit connections and constrained future TOD	Continue study Serves fewer residential areas but need to study an alternative on north side of SR 526	Continue study Fewest potential displacements including on Casino Road community
B	Continue study Convenient for Casino Rd residents, good multimodal connections	Continue study* Concur with CAG recommendation	Continue study Access for historically underserved communities, easy pick up and drop off
C	Remove from study Displaces businesses with fewer potential benefits of station location	Remove from study Concur with CAG recommendation	Remove from study More potential displacements
D	Continue study Serves Casino Rd community while minimizing impacts to businesses	Remove from study Similar to E but with more challenges	Remove from study Potential for more challenging construction
E	Continue to study Good transit connections and future TOD	Continue to study* Concur with CAG recommendation	Continue to study Better connection to buses, most historically underserved communities



*ELG recommended a station and alignment south of SR-526 as the preferred alternative.

I-5 / Broadway Alignment Recommendations

	CAG	ELG	ST Staff
I-5	Preferred Fewer potential displacements and other impacts	Preferred Concur with CAG recommendation	Preferred Fewer potential displacements, no permanent intersection closures
BRD	Continue study To provide reasonable range of alternatives	Continue study To provide reasonable range of alternatives	Continue study To retain multiple options in the DEIS



Everett Station

Recommendations

	CAG	ELG	ST Staff
A	Remove from study Poor connections to downtown	Continue study* Reasonable range of alternatives	Continue study* Best connection to Everett Station, fewest displacements
C	Continue study Compatible with future transportation plans, close to community assets	Preferred Reduced impacts to Broadway, closer to downtown	Preferred Closer to community destinations, higher planned job and population growth
D	Preferred station on McDougall alignment Closer to downtown and historically underserved communities	Preferred station on McDougall alignment Closest to downtown	Preferred station on McDougall alignment Closer to community destinations, higher planned job and population growth

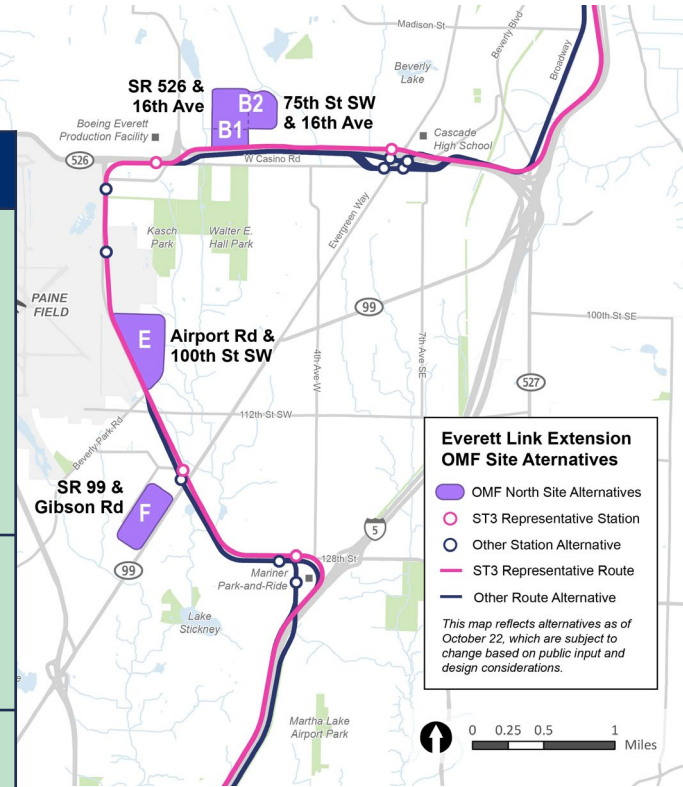


*The ELG recommended a revised Station A that avoids impacting Everett Station

OMF North

Recommendations

	CAG	ELG	ST Staff
Site B1	Continue study of a hybrid location of Sites B1 and B2	Continue study of a hybrid location of Sites B1 and B2	Continue study of a hybrid location of Sites B1 and B2
Site B2	No residential displacements, suits existing land use	To provide reasonable range of alternatives	No residential displacements, least potential to displace historically underserved populations
Site E	Continue study Fewer job and residential impacts	Continue study Concur with CAG recommendation	Continue study Easy connection to mainline track, fewer specialized businesses
Site F	Continue study No wetland impacts	Continue study To provide reasonable range of alternatives	Continue study No identified wetlands or streams, fewer specialized businesses



Next steps

June 2023

- Today - System Expansion Committee considers recommending M2023-47 to the Board of Directors on preferred and other alternatives to study in the Draft EIS
- June 22 - Board of Directors considers adoption of M2023-47

Thank you.



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